Design Guidelines:

Oakmont Village & Eagle Road Corridor

Haverford Township, Delaware County, PA

Comprehensive Plan Addendum

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Town Planners & Landscape Architects

September 27, 2007; October 18, 2007
November 20, 2007; December 12, 2007
January 4, 2008

Design Guidelines: Oakmont Village & Eagle Road Corridor

Haverford Township - Delaware County, PA

Outline of Contents:

September 27, 2007; October 18, 2007; November 20, 2007; December 12, 2007; **January 4, 2008**

Purpose & Intent

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

Buildings Located Close to Sidewalks in Oakmont Village and Setback along Eagle Road

Buildings Grouped Close Together, Oakmont Village

Legislative Intent:

1.1 This Manual is intended to comply with Section 708-A of the Pennsylvania Municipalities Planning Code titled: Manual of Written and Graphic Design Guidelines.

1.2 Placemaking, as described and shown in this Manual, is intended to create a more functional and attractive outcome for the quality of life in Haverford Township in general, and in Oakmont Village and along Eagle Road in particular.

Design Guidelines:

1.3 This Manual shall be utilized to plan, design, construct and maintain buildings, structures, streetscapes, landscapes and hardscapes.

1.4 The places, spaces, buildings, streetscapes, etc. shown and described as "Best Practices" in this Manual shall be emulated.

1.5 All land development and redevelopment plan submissions shall be accompanied by Architectural Plans and Building Elevations that are consistent with this Manual, and the Development Strategy Plans.
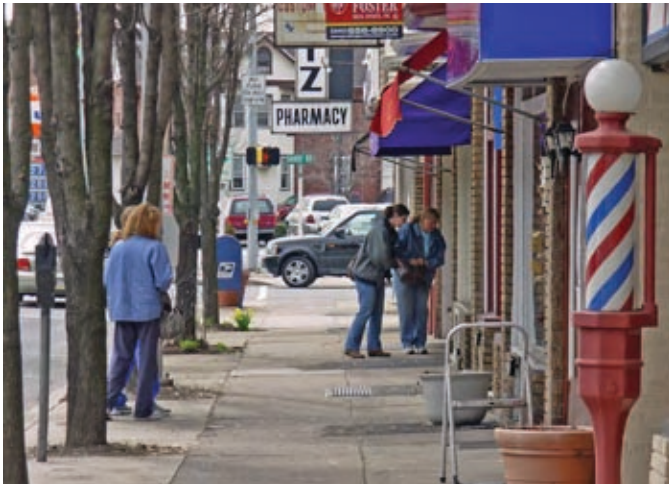

Best Practices example of Street Trees, Media, PA

Street Trees and Buildings Located Close to Sidewalks in Oakmont Village

Streetscape & Street Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

Buildings Adjoining Sidewalk Promote Pedestrian Activity of the Streetscape

Shopfronts in Oakmont Village Enhance the Streetscape Environment

Legislative Intent:

2.1 The Streetscape is intended to be formed by buildings located close to the sidewalk, and/or to be enhanced with such features as street trees, street lights, sidewalks, stoops, and like type amenities.

2.2 Street walls are intended to help form and frame the streetscape and the traditional village character for Oakmont and Eagle Road.

2.3 Street walls are intended to buffer parking.

Design Guidelines:

2.4 The Streetscape shall be embelished with street trees, street lights, sidewalks, fences porches, stoops and other front yard amenities in front of buildings.

2.5 Buildings in alignment shall be maintained and/or provided to form Street Walls.

2.6 Whenever buildings can not form the Street Wall, the options shown on pages 2.2 through 2.5 and in the photo below, shall be utilized.
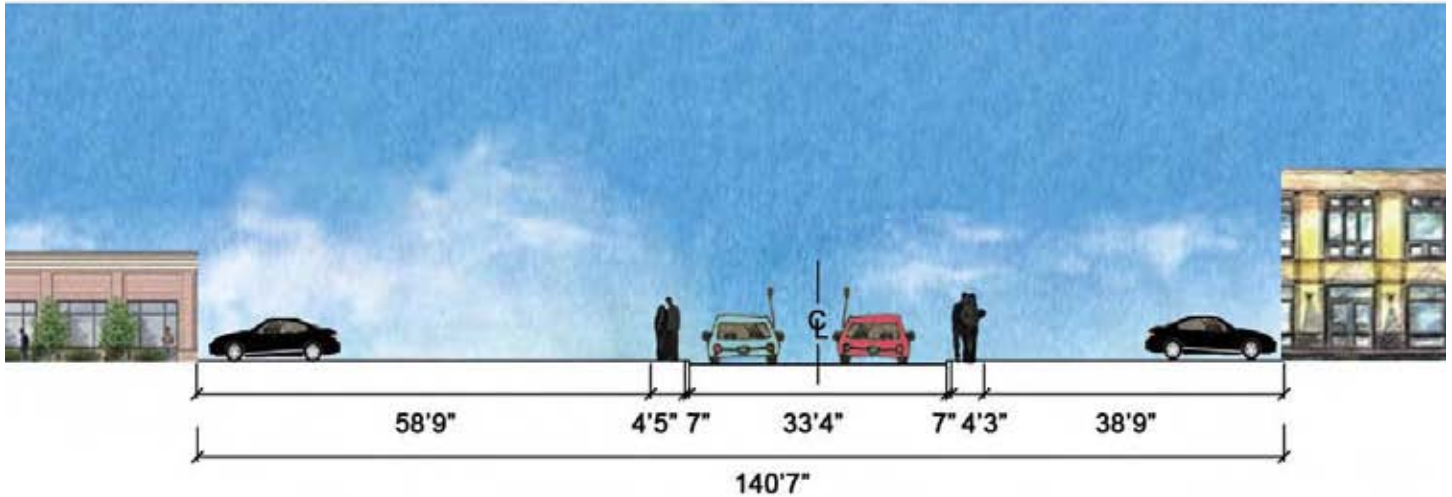

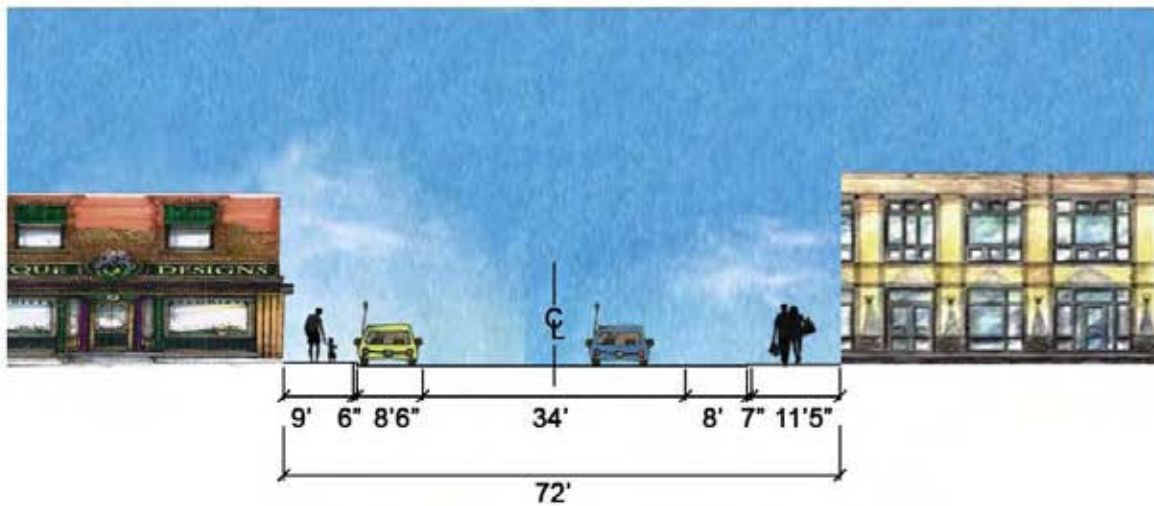Street Wall Located Close to Sidewalk Screens Off-Street Parking along Eagle Road

Best Practices example of Street Wall at Corner Parking Lot, West Chester, PA

Streetscape & Street Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

58'9"   4'5" 7"   33'4"   7" 4'3"   38'9"

140'7"

Typical Eagle Road Street Section
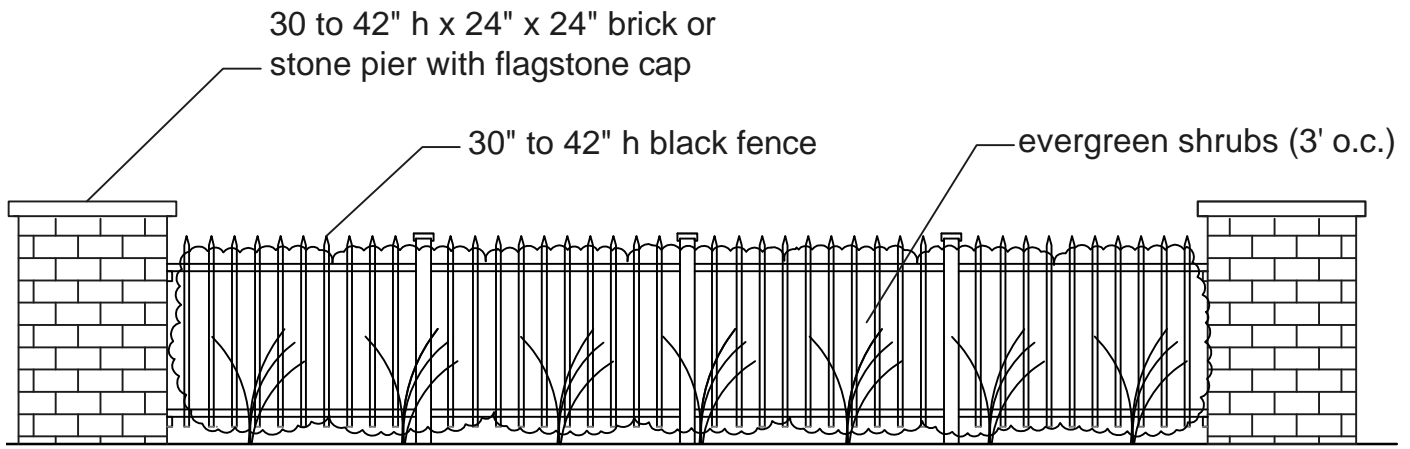

9'   6" 8'6"   34'   8' 7" 11'5"

72'

"Cozy" Oakmont Village Street Section

Note: Whenever the Street Wall to Street Wall dimension is in the 70 to 85 foot range, a more Village type scale and "Main Street" environment exists.

Streetscape & Street Walls

30 to 42" h x 24" x 24" brick or
stone pier with flagstone cap

30" to 42" h black fence

evergreen shrubs (3' o.c.)

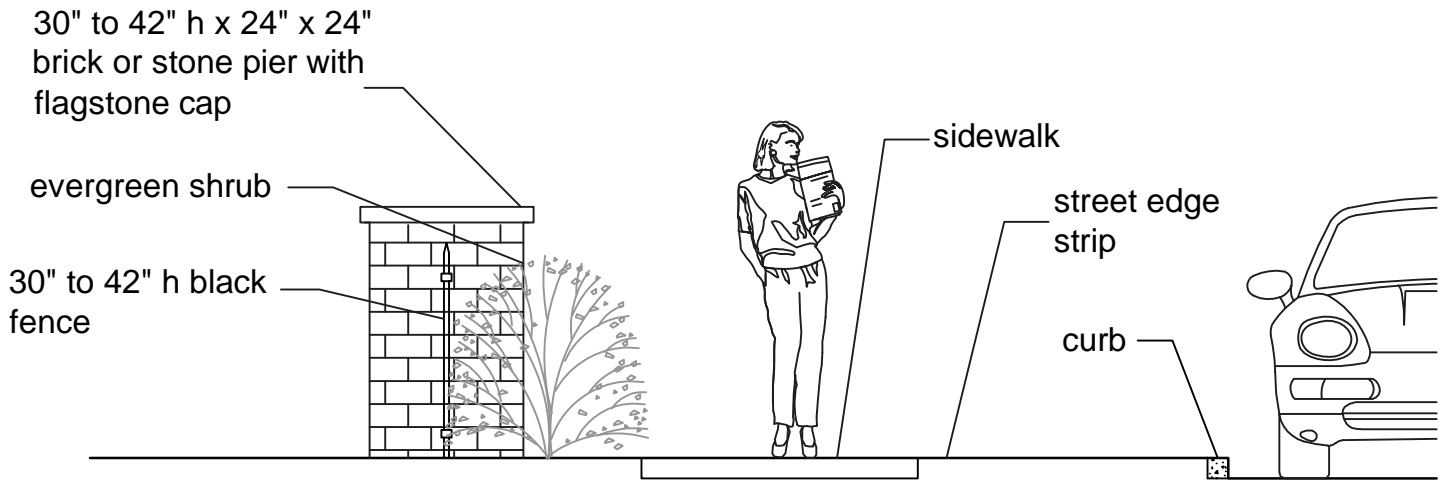**TYPICAL SECTION OF PIER, FENCE,
AND HEDGE**

**BRICK PIER, FENCE, AND HEDGE AS PARKING LOT
SCREEN**

Streetscape & Street Walls

30" to 42" h x 24" x 24"
brick or stone pier with
flagstone cap

evergreen shrub

30" to 42" h black
fence

sidewalk

street edge
strip

curb

TYPICAL ELEVATION OF PIER, FENCE, AND HEDGE

4'

54"

street edge strip

sidewalk

parking lot

21'

30" to 42" h black fence

evergreen shrubs (3' o.c.)

30" to 42" h x 24" x 24" brick or
stone pier with flagstone cap
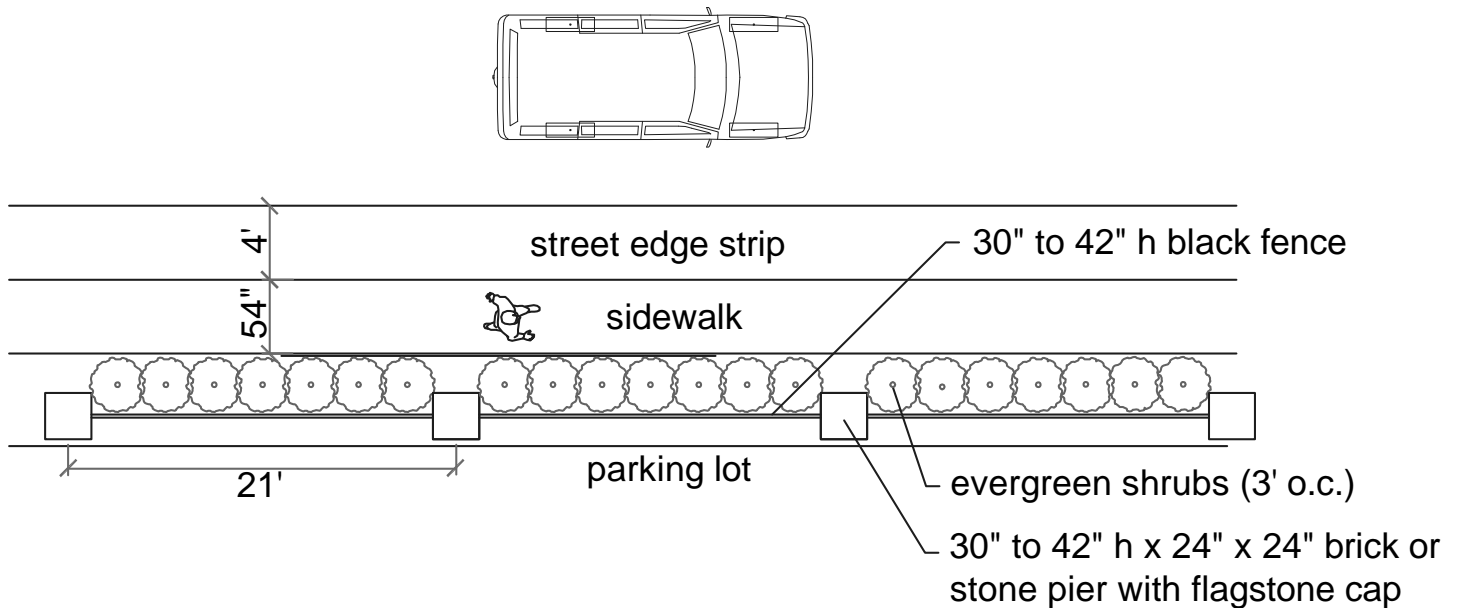
TYPICAL PLAN VIEW OF PIER, FENCE, AND HEDGE

Streetscape & Street Walls

4 - 6' Typ

4"

2' - 6" to 3' - 6"

3' min

8" Dia.

Subgrade

Concrete footing per manufacturers specifications

FENCE DETAIL

24" x 24"

2"

2' - 6" to 3' - 6"

1"

3"

8"

Flagstone cap on 1/2" mortar setting bed

Brick units with 3/8" tooled mortar joints

Concrete masonry units with fully grouted cores and reinforced as required

Aggregate backfill

Typical re-bar

Concrete footing reinforced as required

Prepared subgrade

PIER DETAIL

Streetscape & Street Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

30"

Shrubs to be no less than 30" when planted
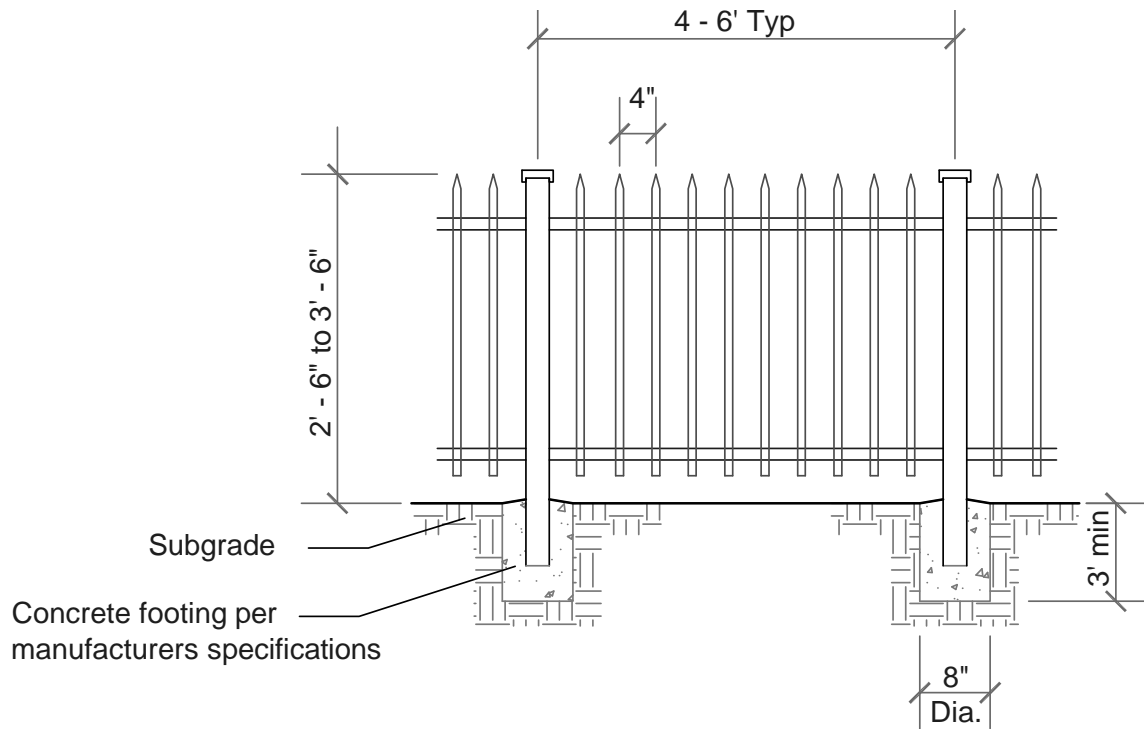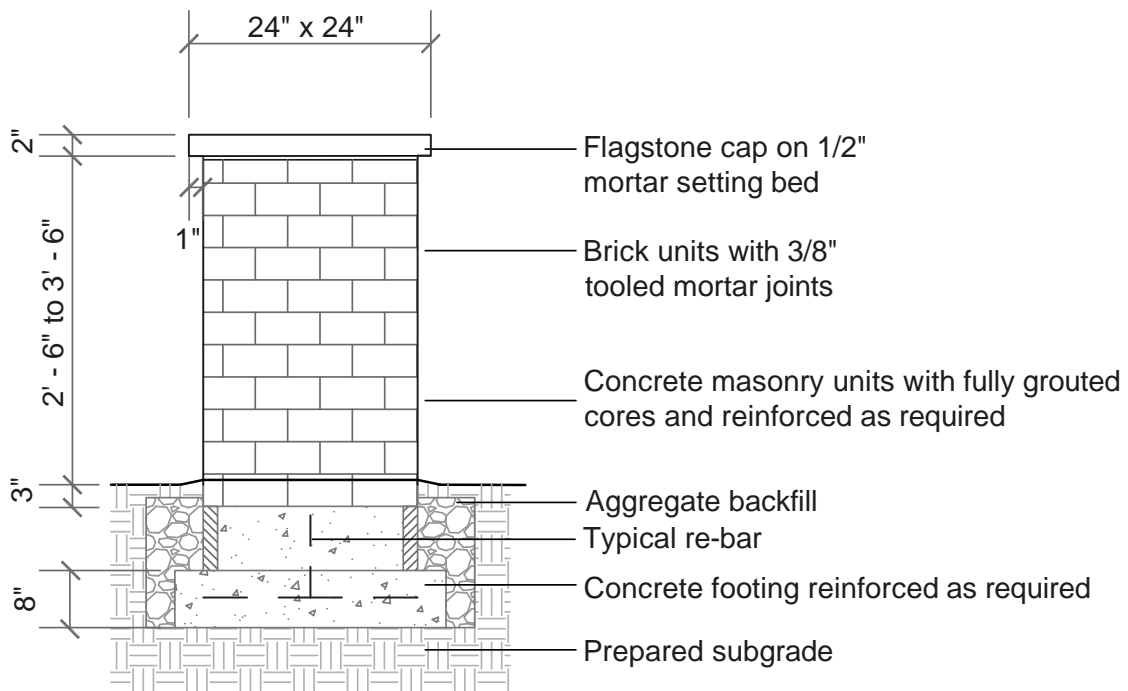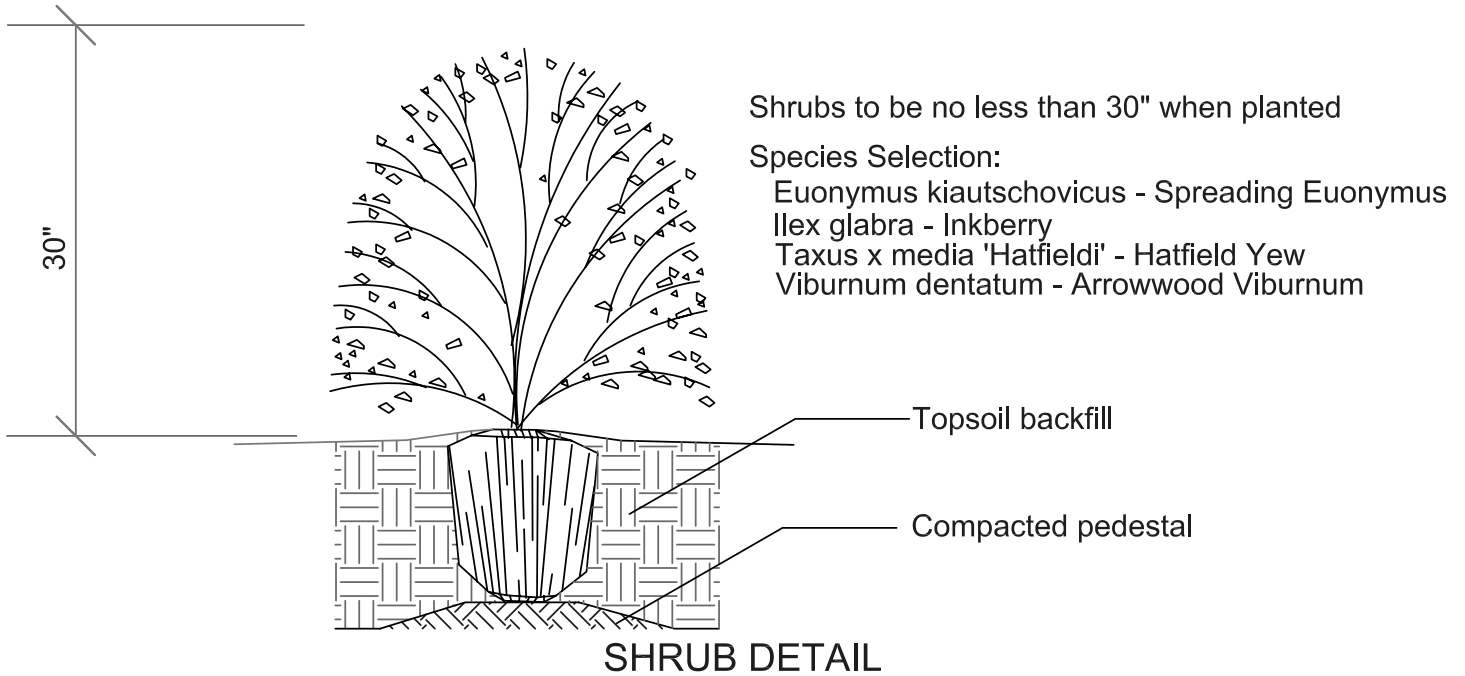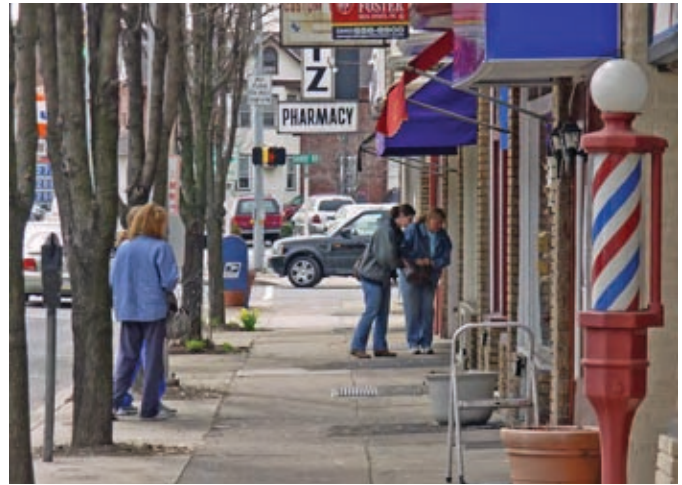
Species Selection:
   Euonymus kiautschovicus - Spreading Euonymus
   Ilex glabra - Inkberry
   Taxus x media 'Hatfieldi' - Hatfield Yew
   Viburnum dentatum - Arrowwood Viburnum

Topsoil backfill

Compacted pedestal

SHRUB DETAIL

Building Location

Buildings Located Close to Sidewalk Help to Create the "Oudoor Room" character in Oakmont Village

Buildings Forming the Build-To Line in Oakmont Village

Legislative Intent:

3.1 Buildings are intended to be located close to the street to help form the Streetscape.

3.2 New buildings are intended to be built and maintained at a "Build-To" Line, in alignment with existing traditional buildings on a block.

Design Guidelines:

3.3 New buildings shall be located in line with existing traditional buildings on a block.

3.4 New buildings shall be located at a Build-To line measured from the edge of the right-of-way of Eagle Road, or within four (4) feet of the average setback of existing buildings on a block.

3.5 If a building cannot be located at a Build-To line, a street wall shall be provided as set forth in the previous Section 2.

Buildings Forming the Build-To Line

Buildings Help to Create the "Outdoor Room" Character of Oakmont Village

TCA

Infill Buildings

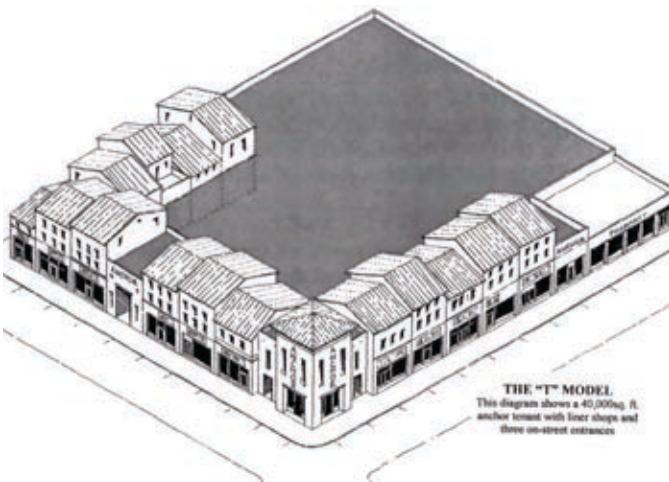Opportunities for Infill Buildings abound South of Oakmont Village

Buildings Anchor Street Corners

Legislative Intent:

4.1 Infill development is intended to be consistent with the existing traditional buildings on the block, in terms of adjacency or proximity of buildings to one another and to the sidewalk.

Design Guidelines:

4.2 Locate new buildings at a Build-To line, as described in the previous Section 3.

4.3 "Anchor" street corners with buildings wherever possible..

4.4 Size new buildings in proportion with traditional buildings in Oakmont Village and along Eagle Road.

4.5 If a larger footprint building is proposed, it should be "edged" with Liner shops.

THE "T" MODEL
This diagram shows a 40,000sq. ft.
anchor tenant with liner shops and
three on-street entrances

Larger Footprint Building or Deck Parking Garage as a Possible Infill Opportunity "edged" by Liner Shops

Existing Traditional Shops

Infill Buildings

New Two-Story Soverign Bank, Lantern Hill, Doylestown, PA

CVS Drugstore and Streetscape, Eagle, PA

Legislative Intent:

4.6   New buildings are intended to complement existing two-story buildings.

4.7   New buildings are intended to promote a village scale and character.

Design Guidelines:

4.9   Maintain existing two-story buildings to enhance mixed-use opportunities.

4.8   Build infill buildings as two-story structures, in context with a village scale environment.

New Two-Story Dunkin Donuts, Claymont, DE

Gas Station / Convenience Store, Meredith, NH

Service Lanes & Alleys

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

New Alley in Gettysburg, PA is example of Best Practice from a redevelopment initiative

Narrow Curb Cut for Alley in Oakmont Village

Legislative Intent:

5.1 Parking accessed from service lanes and alleys is intended to eliminate conventional curb cuts along street frontage, and promote curb appeal along the lot frontage.

5.2 Service lanes and alleys are intended to provide opportunities for the installation of utilities, and for servicing a lot from the rear.

Design Guidelines:

5.3 Service lane and alley widths shall be at least 16 feet in width for two-way travel, and 12 feet for one-way travel.

5.4 Curb cuts for alleys shall be limited to 24 feet in width.

5.5 Alleys shall be landscaped with trees, shrubs, groundcovers, fences, and other features, to add charm and beauty.

5.6 The driveway apron in front of a garage off an alley shall be at least 8 feet in width to allow a vehicle to park parallel to the alley, or 18 feet in length for perpendicular parking.

Frontage Curb Appeal Due to Alley in Rear of Building in Oakmont Village

Alleys Provide Opportunities for Servicing a Lot from the rear in Oakmont Village

TCA

Off-Street Parking

Off-Street Parking Screened with Wall and Trees along Eagle Road

Example of Best Practice for Street Wall of new Wawa in Claymont, Delaware, Buffering Off-street Parking Lot.

Legislative Intent:

6.1　Buildings are intended to screen the parking behind them.

6.2　Existing parking lots that are located in front of buildings are intended to be screened with landscaping and/or walls.

6.3　Off-street parking lots are intended to be generously landscaped.

Design Guidelines:

6.4　New parking shall be located to the rear or side of buildings to the maximum extent possible.

6.5　Off-street parking visible from a street shall be screened with walls, fences, hedges and other landscaping at a minimum of 30 inches in height.

6.6　Parking lot islands and peninsulas shall be located and landscaped to "break-up" expanses of asphalt, whereby no more than fifteen (15) parking spaces shall be in a row (preferably no more than ten (10) in a row) without being interrupted by a landscaped island.
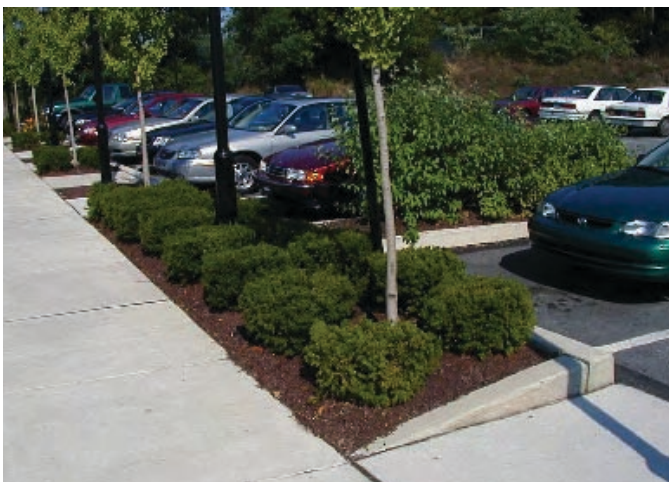
Best Practice example of Parking Lot with Generous Landscaping, Exton, PA
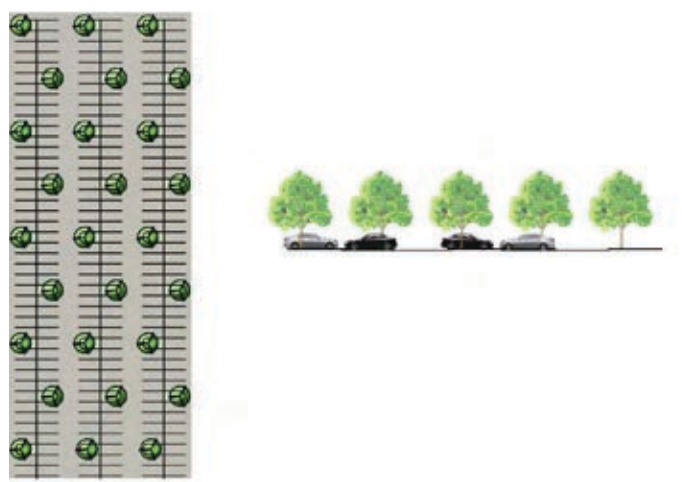
Best Practice example of Shade Trees in an Off-Street Parking Lot

TCA

Curbs & Curb Cuts

Continuous Curb Without Curb Cut in Oakmont

Narrow Curb Cut Minimizes Pedestrian Conflicts in Oakmont Village

Legislative Intent:

7.1   Curb cuts are intended to be limited in width, to help control vehicular access and increase safety for pedestrians.

7.2   Existing wide curb cuts are intended to be "necked-down" to minimize pedestrian conflicts and to provide space for additional landscaping.

Design Guidelines:

7.3   New mid-block curb cuts shall not be created.

7.4   Excessively wide curb cuts shall be narrowed to 24 feet where possible.

7.5   Existing cross streets and service drives and alleys along the rear of the lot shall be utilized to access parking to the maximum extent possible.

7.6   Concrete curbs shall be installed and maintained in accordance with the Haverford Township Subdivision & Land Development Ordinance.

Wide Curb Cuts Should be Narrowed to Provide Increased Safety and Pedestrian Orientation along Eagle Road

Wide Curb Cut Along Eagle Road Increases Vehicular and Pedestrian Conflicts

Mixed Uses

Apartment Above Water Ice Stand, Eagle Road

Apartments Above Commercial along Eagle Road

Legislative Intent:

8.1 New Uses are intended be mixed horizontally or vertically.

8.2 Live-Work Units are intended to be exceptionally good vertical mixed use buildings.

8.3 Mixed Use buildings are intended to provide opportunities for more affordable housing.

Design Guidelines:

8.4 Horizontal and vertical mixed-use buildings should be built to the maximum extent possible.

8.5 Live-Work Units should be provided to the maximum extent possible.

8.6 Offices and/or apartments should also be considered above first floor retail where feasible.

Best Practices example of Ground Floor Cafe, with Apartments Above, Gainesville, FL

Best Practices example of Live-Work Units at Eagleview, Exton, PA

On-Street Parking

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

Best Practice example of On-Street Parking along State Street, Media, PA

On-Street Parking Provides a Buffer for Pedestrians in Oakmont Village

Legislative Intent:

9.1   On-street parking is intended to provide vital overflow parking for patrons and guests.

9.2   On-street parking is intended to provide a useful form of traffic calming.

9.3   On-Street Parking is intended to provide an "insulation" from vehicular traffic for pedestrians along sidewalks.

Design Guidelines:

9.4   On-street parking shall be provided along new streets where feasible.

9.5   On-street parking bays shall be 7 feet wide by 22 feet long.

9.6   On-street parking shall be utilized as guest parking and overflow parking for guests and patrons.

On-Street Parking is a form of Traffic Calming in Oakmont Village

On-Street Parking in Oakmont Village Insulates Pedestrians from Vehicular Traffic

Sidewalks

Pedestrian Friendly Sidewalks Promote Walkability in Oakmont Village

Window Shopping Enhanced by Sidewalks in Oakmont Village

Legislative Intent:

10.1 Sidewalks are intended to provide opportunities for continuous pedestrian circulation.

10.2 Sidewalks are intended to add a pedestrian friendly feature to the Streetscape.

Design Guidelines:

10.2 Sidewalks shall be maintained along both sides of all streets.

10.3 All new sidewalks shall be at least 4' - 6' in width (preferably 5'0" in width) and shall be connected to the buildings on a lot.

10.5 All sidewalks shall be constructed of either brick, poured in place concrete, or other material approved by Haverford Township.
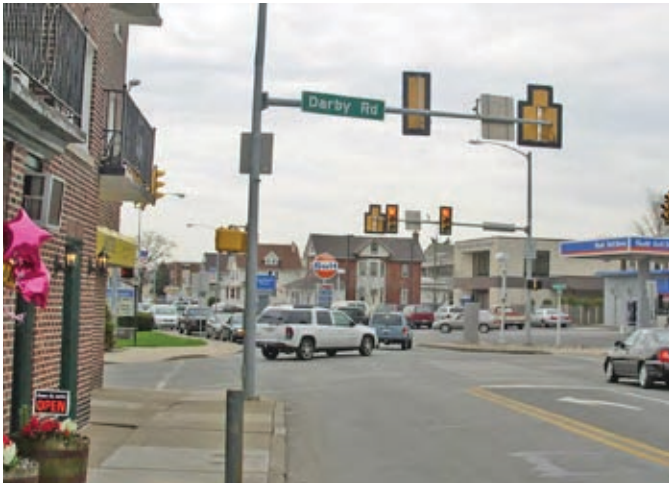

Sidewalks with Small Curb Cuts Reduce Pedestrian and Vehicular Conflicts in Media, PA

Sidewalks with Wide Curb Cuts Diminish Pedestrian Presence

Crosswalks

Well-defined Crosswalks are needed to Continue the Sidewalk System

Best Practices example of Crosswalk, State Street, Media, PA

Legislative Intent:

11.1 Crosswalks are intended to continue the sidewalk system in the Township.

11.2 Crosswalks in driveway throats (at curb cuts) are also intended to continue the sidewalk system.

Design Guidelines:

11.3 Crosswalks shall be at least 6'-0" in width when they cross streets.

11.4 Crosswalks located in driveway throats shall be at least 5'-0" in width.

11.5 Crosswalks shall be unit pavers such as E.P. Henry Mediterranean interlocking pavers or approved equal.

11.6 Where unit pavers can not be installed, zebra stripes shall be painted and maintained.

Best Practice example of Brick Crosswalk in Driveway Throat, West Chester, PA

Well-Defined Zebra-Stripe Crosswalks at Street Intersection, Easton Town Center, OH

Crosswalks

Best Practices example of an At-Grade Crosswalk, West Chester, PA

Mid-Block Crosswalk, Ephrata, PA

Legislative Intent:

11.7   Where possible, crosswalks at street intersections should be at grade to enhance pedestrian curculation.

Design Guidelines:

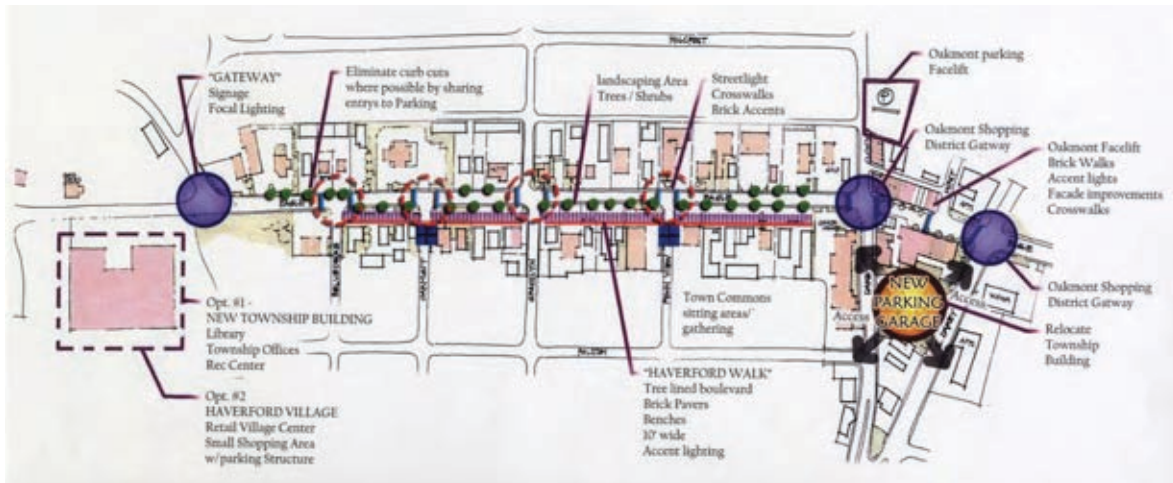11.8   Where approved by Penn Dot, mid-block crosswalks should be signed with pedestrian safety in mind.

Zebra-Stripe Crosswalk

Zebra-Stripe Crosswalk, Philadelphia, PA

Pedestrian Promenade

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

Conceptual Plan for "Haverford Walk", December 2006

Legislative Intent:

12.1 A Pedestrian Promenade is intended to enhance the Eagle Road Corridor for pedestrian use and enjoyment, in conjunction with the redesign of Eagle Road as a boulevard-type street.

Design Guidelines:

12.2 New developemnt, redevelopment, and infill development should be generally consistent with the Conceptual Plan shown above.

12.3 A Pedestrian Promenade at least ten (10) feet in width (preferably 12 feet in width) should be provided and maintained in front of buildings along Eagle Road, in accordance with the Development Strategy Plan.

Pedestrian Promenande at Haile Village Center

Pedestrian Promenade at Main Street at Exton, Exton, PA

Pedestrian Promenade

Boulevard Street, London, U.K.

Pedestrian Promenade and Angled Parking Ardmore, PA

Design Guidelines:

12.4   Portions of Eagle Road should be transformed into a more boulevard-type street in order to better accomodate the Pedestrian Promenade and to create a more attractive Streetscape.

12.5   Angled parking adjoining the pedestrian promenade should be considered to economize on parking spaces.

Design Guidelines:

12.6   The boulevard* should be consistent with Development Stategy Plan 'E', dated February 8, 2008.

*   (see proposed Boulevard cross-section on page 12.2 for complete details)

Boulevard Street, Holland, MI

Boulevard Street, Holland, MI

"Eagle Boulevard" Street Section

"Cozy" Oakmont Village Street Section

Notes:

1.1   See Development Strategy Plan 'E' for Eagle Boulevard concept.

1.2   Whenever the Street Wall to Street Wall dimension is in the 70 to 85 foot range, a more Village type scale and "Main Street" environment exists.

Pedestrian Gathering Places / Pedestrian Pockets
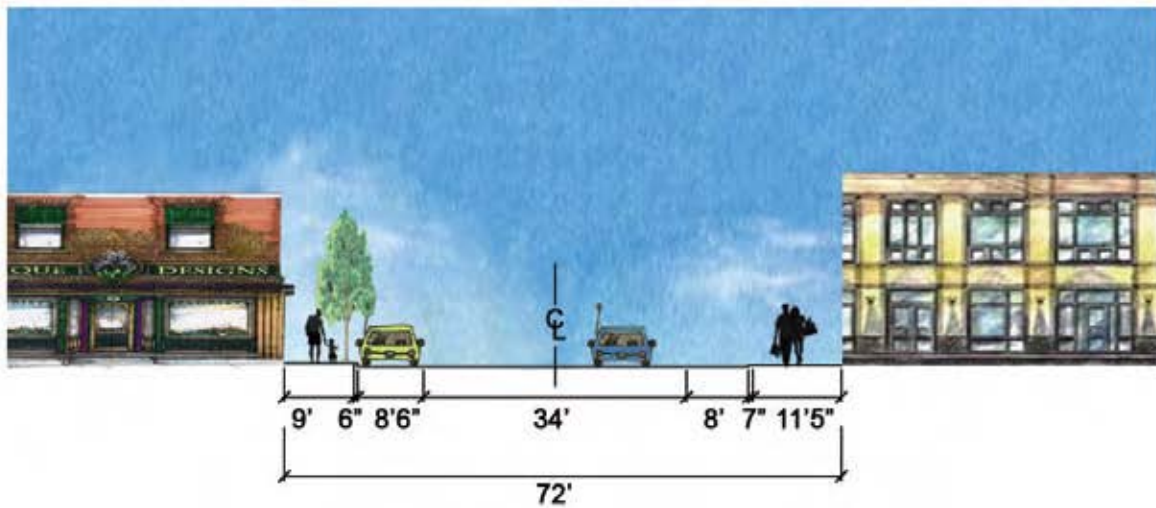
Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

Siting Wall, Newport, RI

Civic Plaza, Cambridge, MA

Legislative Intent:

13.1 Pedestrian gathering places and "pedestrian pockets" are intended to provide viable opportunities to celebrate the public realm.

Design Guidelines:

13.2 Construct and maintain plazas, pavilions, gazebos, and sitting areas as viable pedestrian pockets.

13.3 Install and maintain benches, sitting walls, shade trees, plantings, and lighting in pedestrian areas to enhance attractiveness and safety.

Sitting Area, State College, PA

Civic Plaza, Sewickley, PA

TCA

Building Size, Height & Design

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

Best Practices example of Ground Floor Commercial Liner Shops, Bicentennial Garage, West Chester, PA

Two-Story Buildings with First Floor Retail in Graceful, Narrow-Bay Sequence in Oakmont Village

Legislative Intent:

14.1 The use of traditional materials, such as brick, stone, stucco over stone, and wood siding, is intended to provide a historic reference to the Corridor.

14.2 Facade articulation, variation in roof lines, and vertical expression of buildings, is intended to promote consistency with the scale and proportion of traditional village streetscapes.

14.3 Two-story buildings are intended to promote a better scale and village-type environment.

Design Guidelines:

14.4 Brick, stone, stucco over stone, or wood siding shall be utilized to the maximum extent possible.

14.5 A primarily vertical expression to buildings shall be created through the use of crenalation in the form of windows, doors, pilasters, piers, columns, porches, porticos, stoops, colonnades, and the like.

14.6 Recesses or projections to buildings, from one to four feet, shall be provided whenever the building exceeds 24 feet in width.

14.7 Building heights shall be a minimum of two (2) stories, and a maximum of three (3) stories or 45 feet.

Vertical Expressions to Break Up the Horizontality

Best Practices example of Two-Story Bank, Media, PA

Building Size, Height & Design

Two-Story Buildings with Vertical Bays, the Waterfront, Pittsburgh, PA

Articulated Facade with Vertical Bays, and Articulated Roof, United Savings Bank, Media, PA

Design Guidelines:

14.8   Pilasters shall be extended to the sidewalk as a full vertical unit.

14.9   The roof lines of buildings shall be varied through the use of dormers, gables, turrets, and the like, to the maximum extent possible.

14.10   Facade wall heights of flat-roofed buildings shall be staggered to create visual interest. The heights of such buildings shall vary by at least 42 inches through the use of parapet walls.

14.11   Facades, roof lines, and building heights shall be provided and articulated along the primary facade of a building. On corner lots, all buildings shall have two primary facades, one for each street frontage.

Design Guidelines:

14.12   Building design and materials shall emulate the character of buildings shown in the photographs on pages 14 and 14.1.

14.13   Architectural Plans and Building Elevations shall be submitted with all submissions for land development.

14.14   Minimum 2-story building height regulations are intended to promote less building coverage, and engender a more village-type scale.

New Infill Development with Effective Vertical Pilasters

Facade Articulation Enhances Corner Store Attractiveness at Southern Village, Chapel Hill, NC

Storefronts & Window Displays

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

Best Practices example of Attractive Storefront, Sewickley, PA

Attractive Storefront and Window Display in Oakmont Village

Legislative Intent:

15.1 Attractive storefronts and window displays are intended to enhance the success of businesses.

Design Guidelines:

15.2 Attractive storefronts and window displays shall be created and maintained.

15.3 Awnings should be utilized to the maximum extent possible.

15.4 Window signs should be limited in size to enable better visibility of products for sale.

Best Practices example Of Storefronts at Bicentennial Garage, West Chester, PA

Best Practices example Of Storefronts Main Street at Exton, Exton, PA

Porches, Porticos & Stoops

Porch Provides Shelter for Patrons Along Eagle Road

Best Practices example of Portico at Rolliers Hardware Store, Pittsburgh, PA

Legislative Intent:

16.1   The Porch, Portico, and Stoop is intended to promote a traditional village character, and to serve as a transitional element from the private realm of a building to the public realm of the streetscape.

Design Guidelines:

16.2   New development shall have porches, porticos, or stoops to the maximum extent possible.

16.3   Porches shall be at least 7'-0" deep and 10'-0" wide.

16.4.   Porticos shall be at least 5'-0" deep and 6'-0" wide

16.5.   Porches should not be enclosed.

Enclosed Porch Reduces Building Permeability Along Eagle Road

Best Practices Stoop example at Trader Joe's, Crocker Park, Ohio

TCA

16

Street Trees

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

Effective Street Tree Spacing in Oakmont Village

Street Trees Help to Define Public Realm in Oakmont Village

Legislative Intent:

17.1   Street trees are intended to provide shade and screening, and add a graceful component to the streetscape.

17.2   When placed in a regular opposite alignment along both sides of streets, street trees are intended to form an Allee effect.

Design Guidelines:

17.3   Street trees shall be installed and maintained along both sides of all streets.

17.4   One (1) street tree shall be provided for at least every fifty (50) linear feet of street frontage.

17.5   Street trees, at the time of planting, shall be no less than three to three and one half inches (3"–3½") in caliper and shall be in accordance with the latest edition of the "American Standard for Nursery Stock," of the American Association of Nurserymen.

Best Practices Street Trees example along Sidewalk, Mashpee, Mass.

Best Practices Street Tree example, State College, PA